

**§9-5 FORM-BASED CODE PROVISIONS**

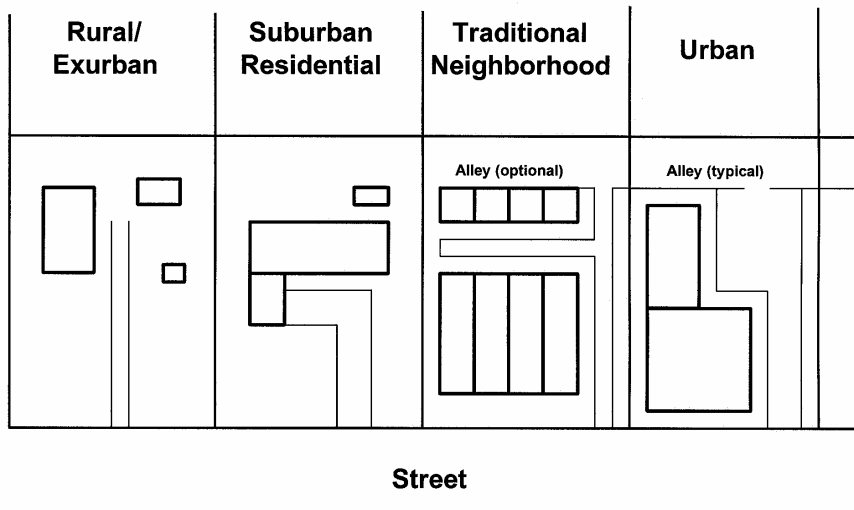
- §9-5-1 CHARACTER AREA REGULATING PLAN
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**[See Commentary]**

§9-5-1 CHARACTER AREA REGULATING PLAN

The future development map, as adopted in the Comprehensive Plan of the County [City] is hereby adopted by reference as the organizing framework for these regulations. The future development map divides the jurisdiction into character areas which consist of the following:

1. Urban (URB)
2. Traditional Neighborhood (TND)
3. Suburban Residential (SUB) (see commentary)
4. Rural/Exurban (R-EX)



**[See Commentary]**

§9-5-2 DEFINITIONS

**[See Commentary]**

Arcade: A covered passage contiguous to a street or plaza with shops on one or both sides, accessible to the public. Generally, the façade overlaps the sidewalk while the shopfront remains set back. The sidewalk is fully covered with an overhang.

**Architectural appearance, exterior:** The architectural character and general composition of the exterior of a structure, including but not limited to the kind, color, and texture of the building material and the type, design, and character of all windows, doors, light fixtures, signs, and appurtenant elements.

**Architectural features:**

Ornamental or decorative features attached to or protruding from an exterior wall or roof, including cornices, eaves, belt courses, sills, lintels, bay windows, chimneys, and decorative ornaments.

**Awning:** A roof-like cover, often made of fabric or metal, designed and intended for protection from the weather or as a decorative embellishment, and which projects from a wall or roof of a structure over a window, walk, or door.

**Block:** An area of land bounded (surrounded on all sides) by streets, or by a combination of streets and public land, railroad rights-of-way, utility right-of-way, waterways, or any other barrier to the continuity of development.

**Block width:** The distance as measured along rear property lines between intersecting streets.

**Build-to line:** An alignment establishing a certain distance from the property line (street right-of-way line) along which the building is required to be built.

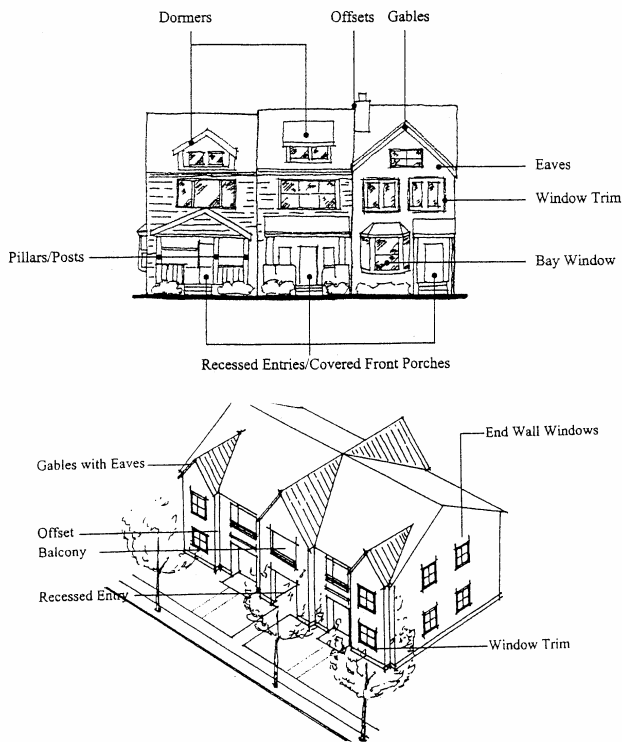
**Building frontage:** The length of any side of a building which fronts on a public street, measured in a straight line parallel with the abutting street.

**Character:** Special physical characteristics of an area or structure that set it apart from its surroundings and contribute to its individuality.

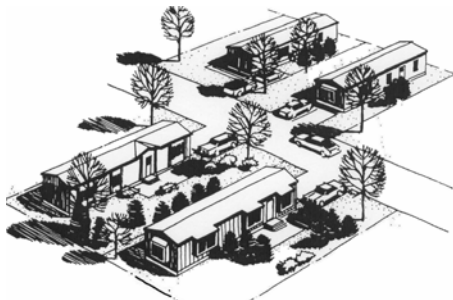
**Cornice:** Any horizontal member, structural or non-structural, of any building, projecting outward from the exterior walls at the roof line.

**Court:** An open, uncovered area partly or wholly enclosed by buildings or walls and used primarily for supplying access, light, air, and/or open space to abutting buildings.

**Dwellings, types of:** See illustrations below:

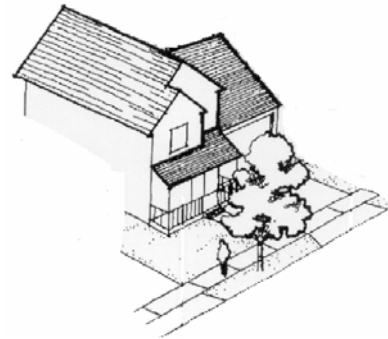


Source: Oregon Transportation and Growth Management Program. Commercial and Mixed Use Development Code Handbook.



Source: *Time-Saver Standards for Housing and Residential Development*. 2nd Ed. Joseph De Chiara, Julius Panero, and Martin Zelnik, Editors. New York: McGraw-Hill Professional, 1995. Chapter 11, Figure 17, p. 977.

**Manufactured Home**



**Detached Single-family Dwelling**

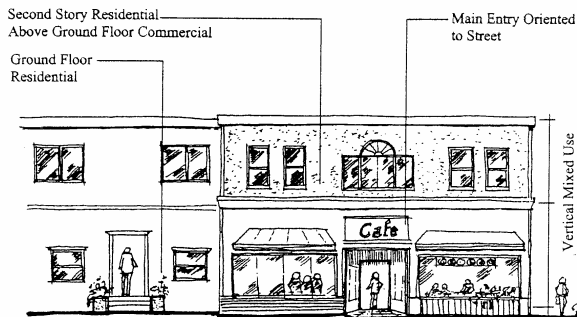


Source: John Matusik and Daniel Deible. "Grading and Earthwork." Figure 24.30 in *Land Development Handbook*, 2<sup>nd</sup> ed. New York: McGraw-Hill, 2002, p. 571.

**Townhouse**

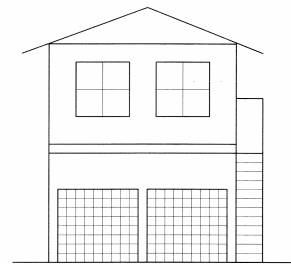


**Apartment or Condominium**



Source: Oregon Transportation and Growth Management Program. Commercial and Mixed Use Development Code Handbook.

**Residence as Part of Mixed-Use Building**



Accessory Apartment, Detached  
 (Above Garage Shown)

**Accessory Apartment**

**Exurban:** An area that lies beyond an urban area and its suburbs.

**Façade:** The face (exterior elevation) of a building, especially the face parallel to or most nearly parallel to a public street.

**Floor area:** The sum of the gross horizontal areas of each floor of the principal building, and any accessory buildings, measured from the exterior walls or from the center line of party walls. The term does not include any area used exclusively for the surface parking of motor vehicles or for building or equipment access, such as stairs, elevator shafts, and maintenance crawl space.

**Floor area ratio (FAR):** The total floor area of the building or buildings on a lot or parcel divided by the gross area of the lot or parcel.

**Green:** An open space available for unstructured recreation, its landscaping consisting of grassy areas and trees.

**Mixed-use development:** A single building containing more than one type of land use; or a single development of more than one building and use, where the different types of land uses are in close proximity, planned as a unified complementary, cohesive whole.



Source: Abbey Deiss, Jerry Weitz & Associates, Inc.

**Parapet:** That portion of a wall which extends above the roof line.

**Pedestrian connection:** A continuous, unobstructed, reasonably direct route between two points that is intended and suitable for pedestrian use. Pedestrian connections include but are not limited to sidewalks, walkways, accessways, stairways, and pedestrian bridges.



Source: Pedestrian Bridge, Fernwood Development in DeKalb County, Georgia. Courtesy Atlanta Regional Commission, Land Use Division.

**Pedestrian-oriented development:** Development designed with an emphasis primarily on the street sidewalk and on pedestrian access to the site and building, rather than on auto access and parking areas. The building is generally placed close to the street and the main entrance is oriented to the street sidewalk. There are generally windows or display cases along building facades which face the street.

**Plaza:** An open area adjacent to a civic or commercial building that functions as a gathering place and may incorporate a variety of non-permanent activities, such as vendors and display stands.

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**Porch:** A projection from a building wall which is covered but enclosed on no more than one side by a vertical wall.

**Retail display window:** A window or opening in the exterior wall of any portion of a building used for business purposes, through which merchandise, services, or businesses are displayed or advertised and visible from the ground or sidewalk level.

**Rural:** A sparsely developed area where the land is primarily used for farming, forestry, resource extraction, open space, and/or low-density residential uses (one unit or less per acre).

**Shopfront:** A business or retail use where the façade is aligned directly on the frontage line with the entrance at grade; typical of sidewalk retail. Shopfronts often have awnings.



Source: Photo from Lithonia, Georgia, Courtesy Atlanta Regional Commission, Land Use Division.

**Square:** An area for passive recreational use, usually bounded by streets.

**Stoop:** An exterior floor, typically but not necessarily constructed of concrete and/or masonry, with a finished floor elevation at least six inches higher than the adjacent ground level, and utilized primarily as an access platform to a building.

**Streetscape:** An area that may either abut or be contained within a public or private street right-of-way or accessway that may contain sidewalks, street furniture, trees and landscaping, and similar features. Streetscape also includes the visual image of a street, including the combination of buildings, parking, signs, and hardscapes.



Source: Norcross, Georgia, courtesy Atlanta Regional Commission, Land Use Division.

Traditional neighborhood development: An approach to land-use planning, urban design, and development that promotes the building of neighborhoods with a mix of uses and housing types, architectural variety, one or more central public gathering places, interconnected streets (and sometimes alleys), and edges defined by greenbelts or boulevards. The basic goal is the integration of residences with work, shopping, recreation, and transit all within walking distance.



Source: Silicon Valley Network. 2001. Silicon Valley Joint Venture. Imaging by Urban Advantage. [www.urbanadvantage.com](http://www.urbanadvantage.com).

Urban: Of, or relating to, characteristic of, or constituting a city. Urban areas are generally characterized by moderate and higher density residential (i.e., usually three or more dwelling units per acre), civic/institutional and commercial/industrial development, including supporting public services such as water, sewer, and roads.

Viewshed: The area within view from a defined observation point or corridor.

Village: A small, compact center of predominantly residential character but with a core of mixed-use commercial, residential, and community services. A village typically has a recognizable center, discrete physical boundaries, and a pedestrian scale and orientation.

Workplace: A place of employment, base of operation, or predominant location of an employee.

### §9-5-3 USE PROVISIONS

§9-5-3.1 Permitted, Exception, and Excluded Uses. Uses shall be regulated as specified in Table 1. Uses listed as “permitted” are allowed outright (with not discretionary review). Uses listed as “exception” require discretionary review (i.e., a special or conditional use). Uses listed as “excluded” are prohibited in that character area.

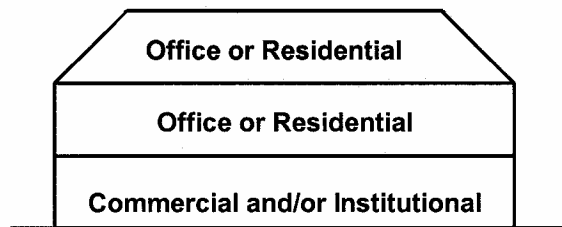
TABLE 1  
 USE PROVISIONS

Use	Rural/ Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Agriculture	Permitted	Excluded	Excluded	Excluded
Manufactured home	Permitted	Excluded	Excluded	Excluded
Detached, single-family	Permitted	Permitted	Permitted	Exception
Attached, single-family (townhouse)	Excluded	Excluded	Permitted	Permitted
Residential condominium or apartment (freestanding)	Excluded	Excluded	Permitted	Exception
Residence as part of mixed-use building	Excluded	Excluded	Permitted	Permitted
Accessory apartment	Exception	Exception	Permitted	Permitted
Civic and institutional	Permitted	Exception	Permitted	Permitted
Office and professional	Excluded	Excluded	Permitted	Permitted
Service and retail	Excluded	Excluded	Permitted	Permitted
Industry	Excluded	Excluded	Excluded	Exception
Other (unspecified)	Exception	Exception	Exception	Exception

**[See Commentary]**

§9-5-3.2. Uses on Shopfront Streets.

Within buildings fronting along designated “shopfront” streets in Urban and Traditional Neighborhood Development character areas only commercial, office, civic, or institutional uses shall occupy the ground-level floor. Floors above the ground level may be occupied by office or residential uses.



§9-5-4 BUILDING REQUIREMENTS

§9-5-4.1 Building Height. The height of buildings shall meet the specifications of Table 2.

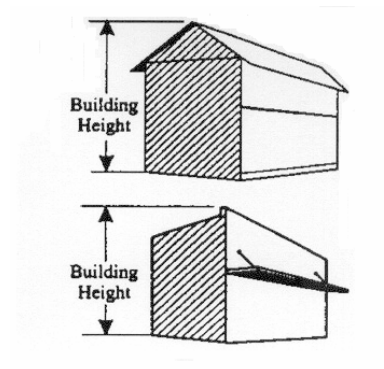


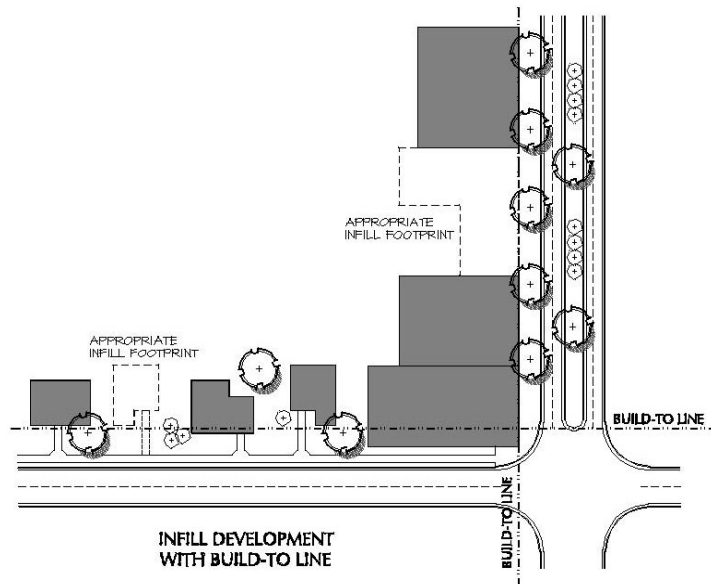
TABLE 2  
 BUILDING HEIGHT

Building Height	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Maximum	Home: 2 stories; Agriculture: 75'	Home: 3 stories	Home: 4 stories	Mixed-Use Building: 6-10+
Minimum	None	None	2 stories	3 stories

**[See Commentary]**

§9-5-4.2 Building Lines.

Buildings shall observe the setbacks or “build-to” lines requirements specified in Table 3.



Source: The Jaeger Company.

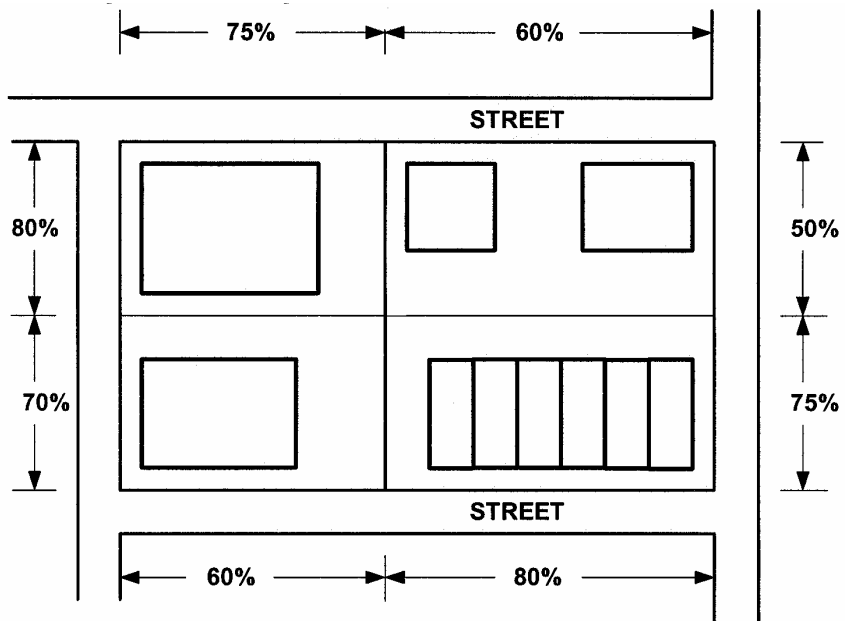
TABLE 3  
 BUILDING SETBACKS AND BUILD-TO LINES

Building Line	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Build-to Line	May be required	None	10' to 15'	0' to 10'
Front, min. (ft.)	70'	35'	10'	None
Front, max. (ft.)	None	None	20'	15'
Side, min. (ft.)	40'	10'	10'	None
Rear, min. (ft.)	50'	20'	10'	0' to 20'
Rear, abutting alley (ft.)	Not applicable	Not applicable	None	None

**[See Commentary]**

§9-5-4.3 Minimum Building Frontage.

The minimum percentage of the lot's street frontage that must be occupied by one or more principal buildings shall be as provided in Table 4.



**Illustrative Building Frontages**

TABLE 4  
 MINIMUM BUILDING FRONTAGE

	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Minimum Building Frontage (%)	None	None	40-50%	60-80%

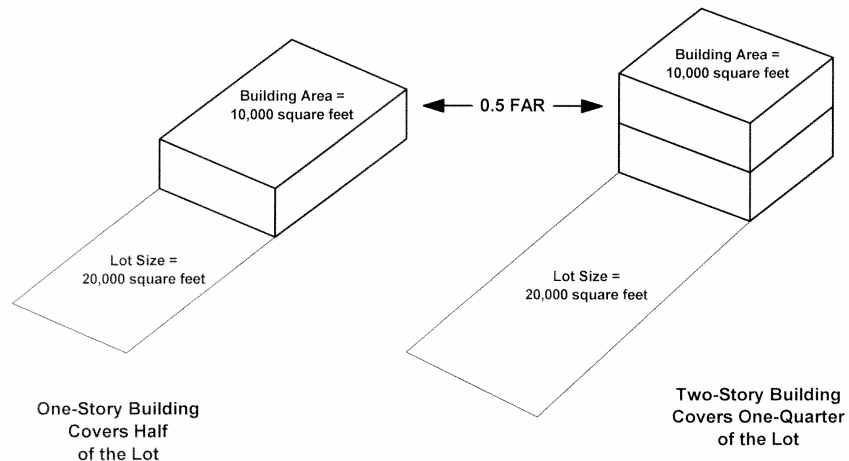
**[See Commentary]**

§9-5-4.4 Building Intensity. The intensity of buildings shall not exceed floor-area ratios specified in Table 5.

TABLE 5  
 MAXIMUM BUILDING INTENSITY

	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Maximum floor-area ratio	None	None	0.4-0.6	0.8-10.0+
Maximum Residential Density	1 unit per 2-25 acres	1 unit per 25,500- 43,560 square feet	5-8 units per acre (1 unit per 6,000 sq. ft. typ.)	FAR of 0.4 to 5.0 (included within max. FAR)

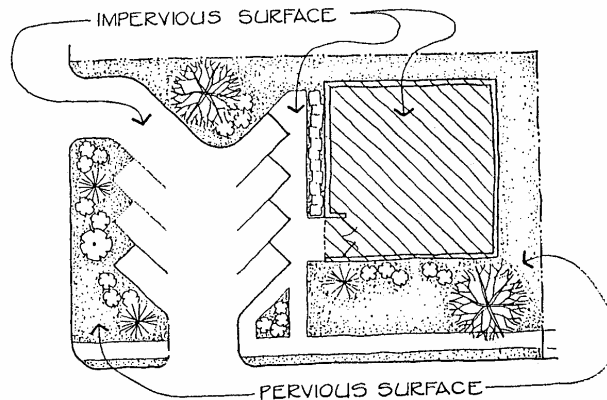
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**Floor Area Ratio**

**[See Commentary]**

**§9-5-4.5 Maximum Impervious Surface Coverage.** Impervious surfaces shall not exceed the maximum impervious surface percentages (calculated on the basis of the lot) specified in Table 6.



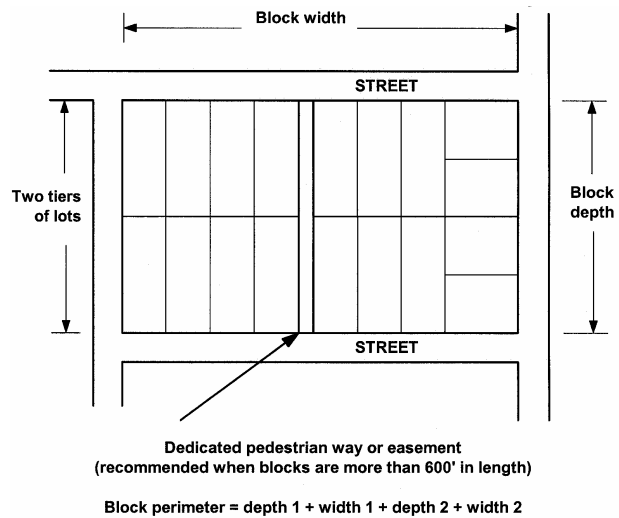
Source: Davidson, Michael, and Fay Dolnick. *A Glossary of Zoning, Development, and Planning Terms*. PAS Report No. 491/492 (Chicago, American Planning Association, 1999, p. 126).

**TABLE 6  
MAXIMUM IMPERVIOUS SURFACE COVERAGE**

	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Max. impervious surface coverage (% of lot)	5-15%	12-25%	40-65%	70-90%

**[See Commentary]**

§9-5-4.6 Maximum Block Sizes.  
 The width of blocks, and block perimeters, shall not exceed the specifications of Table 7.



**Block Width, Block Depth, and Block Perimeter**

**TABLE 7  
 MAXIMUM BLOCK WIDTH AND BLOCK PERIMETER**

	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Maximum Block Width (ft.)	None	800-1,000'	600'	300-500'
Maximum Block Perimeter (ft.)	None	3,200-5,000'	2,400-3,000'	1,200-1,800'

**[See Commentary]**

§9-5-4.7 Recreation and Open Space. Recreation and open space shall consist of minimum areas and of the types of recreation and open space specified in Table 8.

**TABLE 8  
 RECREATION AND OPEN SPACE REQUIREMENTS**

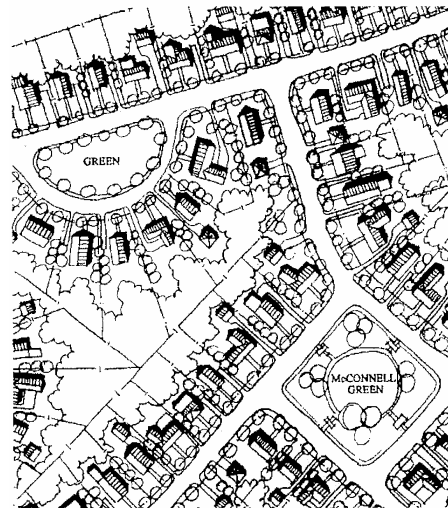
	Rural/Exurban (R-EX)	Suburban Residential (SUB)	Traditional Neighborhood (TND)	Urban (URB)
Minimum Percent of Lot Devoted to Open Space	None	0-15%	15-30%	5-15%
Location of open space	Within lots and farmsteads (private)	Within lots (private) and/or in community recreation facilities, parks,	Pocket parks, greens, and squares	Pocket parks, greens, squares, and urban plazas

		and playgrounds	
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Source: City of Raleigh Urban Design Guidelines.

**Illustrative Urban Area Green**



Source: Arendt, Randall. 1999. *Crossroads, Hamlet, Village, Town: Design Characteristics of Traditional Neighborhoods, Old and New*. PAS Report No. 487/488, Figure 109, p. 67. Chicago: American Planning Association.

**Illustrative Greens, TND**

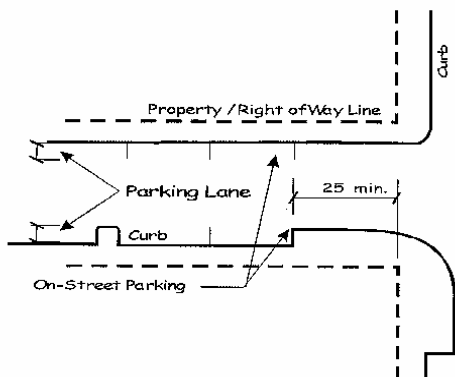
**[See Commentary]**

§9-5-5 PARKING

§9-5-5.1 Off-Street Parking Requirements. Parking requirements are provided in Section 7.4 of this Code.

**[See Commentary]**

§9-5-5.2 On-street Parking. On-street parking shall be permitted on all streets within Urban and Traditional Neighborhood character areas, except within twenty-five feet of the right-of-way of an intersecting street. On-street parking is required in all Urban and Traditional Neighborhood character areas designated as storefront streets.

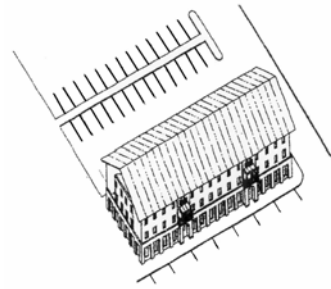


Source: Kendall, Florida. Ordinance 99-166. Downtown Kendall Urban Center District.

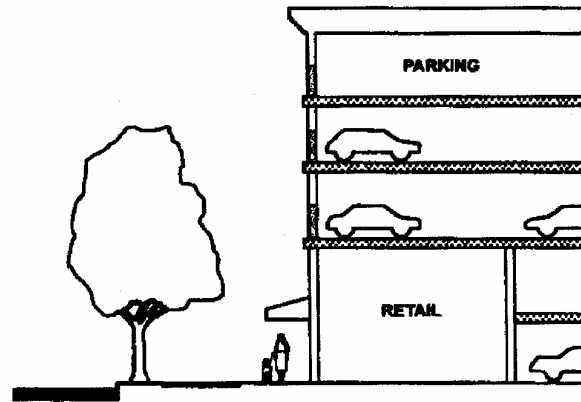


Source: Calthorpe, Peter. "The Regional City." In *Time-Saver Standards for Urban Design*, edited by Donald Watson, Alan Plattus, and Robert Shibley. New York: McGraw-Hill, p. 1.9-5.

§9-5-5.3 Placement of Off-Street Parking. Where off-street parking is required in Urban and TND character areas, it shall be not be placed between the street right-of-way and the building fronting the street.



§9-5-5.3 Parking Decks. Above-ground parking structures, if they must front on a shopfront street, shall provide continuous street-fronting ground level commercial, office, or residential spaces and uses, except at ingress and egress points into the structure.



**PARKING DECK**

Source: City of Atlanta. 2000. City of Atlanta Neighborhood Commercial Zoning District Regulations.

§9-5-6 STREET REQUIREMENTS

*[See Commentary]*

**Commercial Street**

- Traffic lanes: Two ways
- Parking lanes: Both sides
- Right of-way: 60 feet
- Pavement width: 24-36 feet
- Curb type: Raised
- Curb radius: 15 feet
- Sidewalk width: 6-10 feet
- Planter width: 4 feet
- Planting: Trees in wells, continuous 30' o.c.

**Alley**

- Traffic Lanes: Two ways
- Parking lanes: None
- Right-of-way: 24 feet
- Pavement width: 20 feet
- Curb type: Rolled
- Curb radius: 15 feet
- Sidewalk width: None
- Planter width: None
- Planting: None

**TND Residential Street  
 (see illustration)**

- Traffic Lanes: Two ways
- Parking lanes: Both sides
- Right of-way: 60 feet
- Pavement width: 24 to 38 feet (where parking

**Rural Lane**

- Traffic Lanes: Two ways
- Parking lanes: None
- Right-of-way: 30 feet
- Pavement width: 20 feet

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is permitted)

Curb type: Raised

Curb radius: 15 feet

Sidewalk width: 4-5 feet

Planter width: Varies

Planting: Trees in strip w/ variable spacing

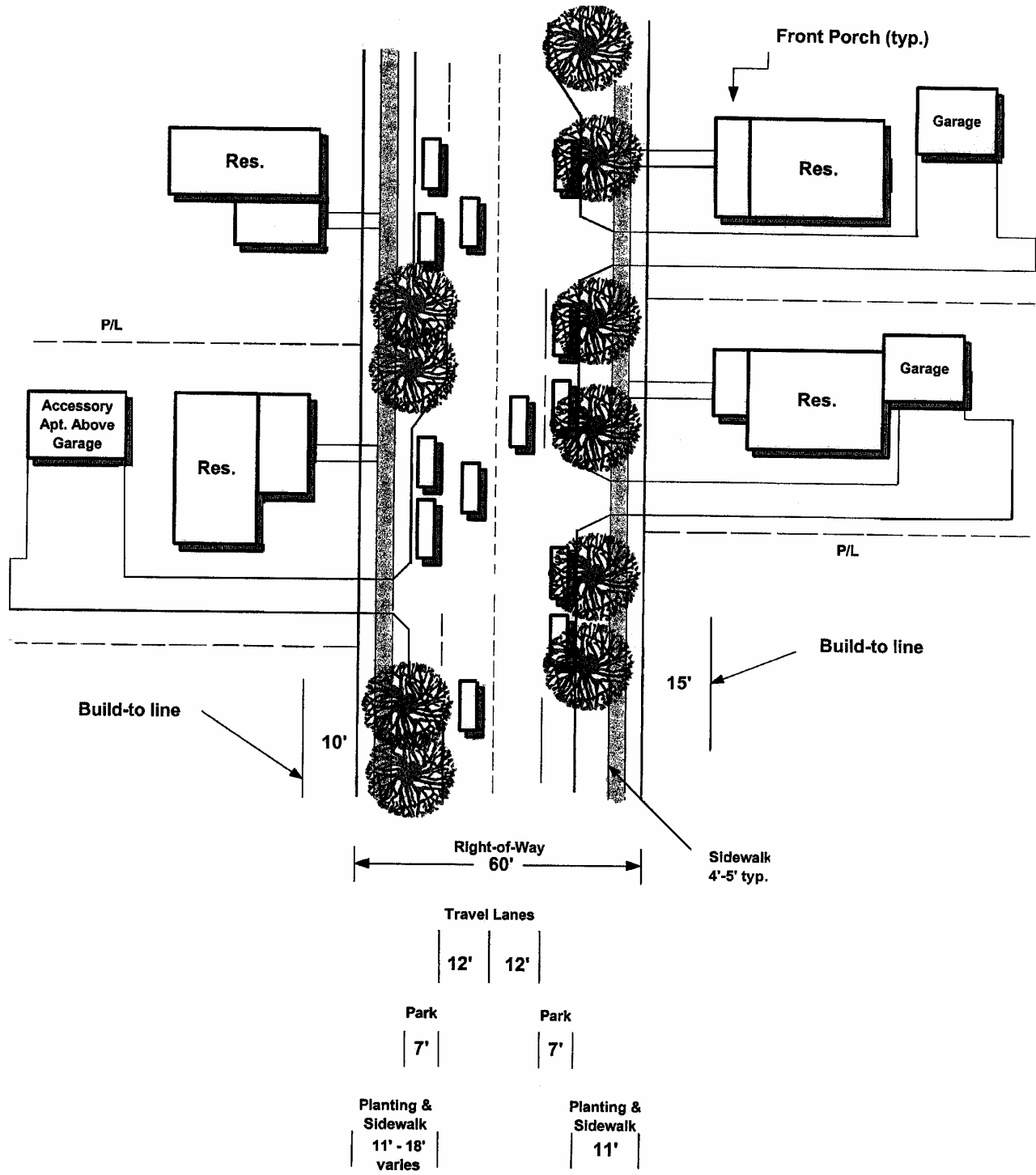
Curb type: Rolled/ Asphalt

Curb radius: 15 feet

Sidewalk width: None (gravel walk optional)

Planter width: None

Planting: None

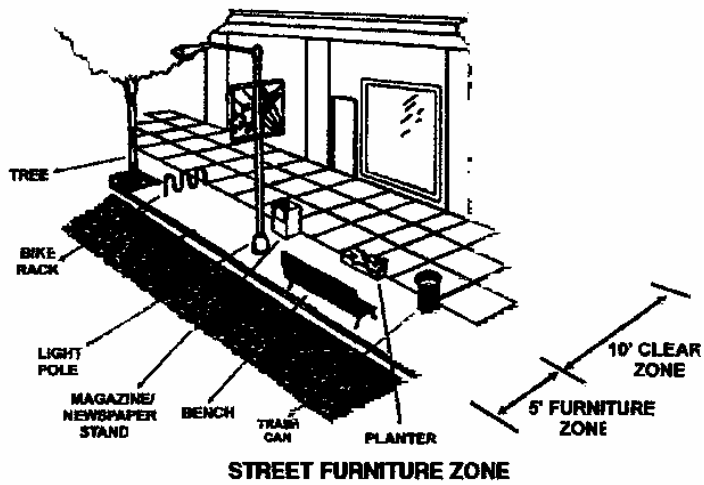


TND Residential Street

§9-5-7 STREETSCAPE REQUIREMENTS

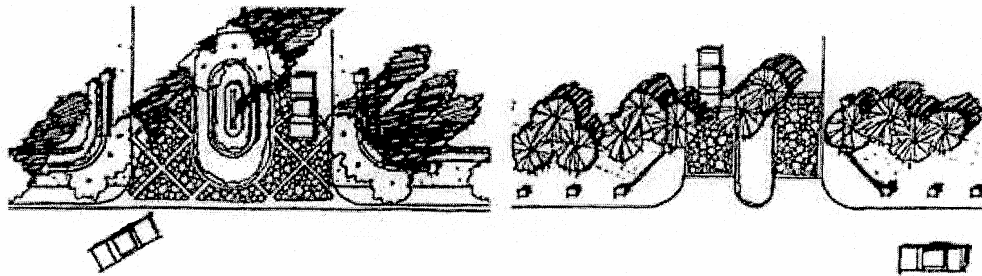
§9-5-7-1 Sidewalk Clear Zone and Street Furniture Zone

In shopfront areas, the street sidewalk shall consist of a minimum ten foot wide (10') clear zone. Between the clear zone and street curb, there shall be a five-foot wide street furniture zone for the placement of light poles, benches, bicycle parking, trash receptacles, tree planting, and planter boxes.



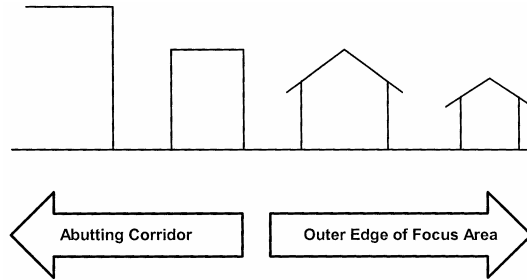
Source: City of Atlanta. 2000. City of Atlanta Neighborhood Commercial Zoning District Regulations.

§9-5-7-2 Pedestrian Crossings of Driveways and Intersections. The color and composition of sidewalk shall be continued as it crosses vehicular driveways and street intersections. Where the sidewalk color and composition of the sidewalk is not otherwise distinguishable from the driveway or roadway composition and color, the sidewalk shall be composed of material with color and texture that demarcates the pedestrian crossing. This provision is required to promote safety of pedestrians, as different textures or colors of pedestrian crossings alert or remind the motorist pedestrians are present.



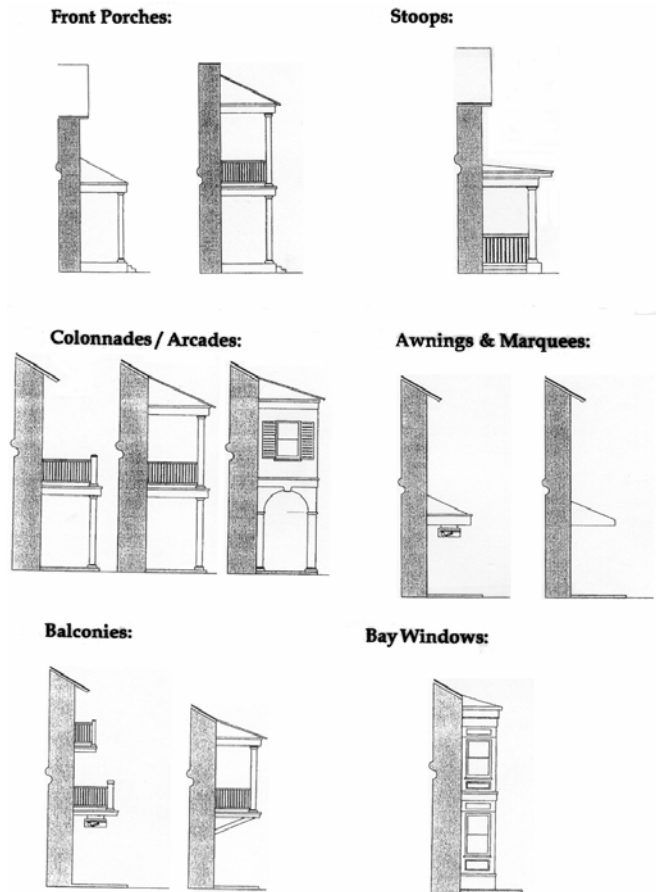
§9-5-8 ARCHITECTURAL REQUIREMENTS

§9-5-8.1 Height and Mass. For activity centers, building heights should be greatest near the center of these developments and transition to lower heights outward toward the edge of the development. Buildings at the edge should be comparable in height and massing to the adjacent and nearby properties as well as the surrounding neighborhood.



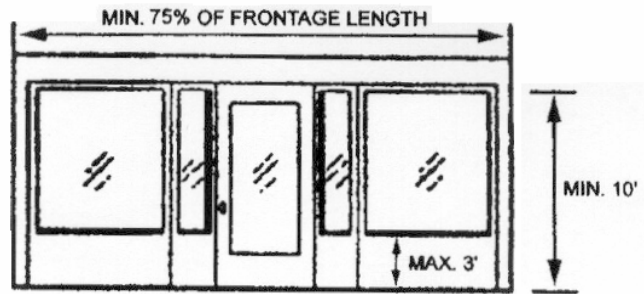
**Decrease Height and Mass  
 in the Focus Area**

§9-5-8.2 Front Façades. Buildings in Urban and TND character areas shall have a front porch, stoop, colonnade, awning, balcony, or bay window for a length of between 25 percent (25%) and 100 percent (100%) of the building frontage. Bay windows shall have a minimum depth of four feet (4'), balconies shall have a minimum depth of six feet (6') and porches, stoops, colonnades, and awnings shall have a minimum depth of eight feet (8').



**Illustrative Front Facades**

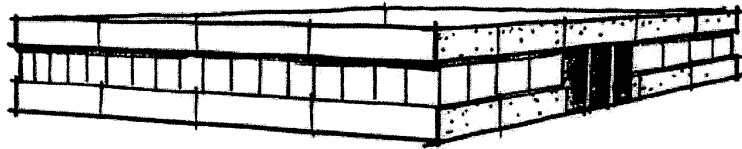
§9-5-8.3 Shopfront Windows.  
In designated shopfront areas, at least 75 percent of the building's front façade shall consist of clear glass window and/or door. Windows shall not be at least ten feet (10') in height and no more than three feet (3') above the sidewalk.



§9-5-8.3. Rooflines.  
Roof planes should be varied to increase visual interest, and awnings above windows and entrances also help to provide visual interest.



Flat roofs and lengthy, non-articulated walls are discouraged.



§9-5-8.4 Rooftop Mechanical Equipment. Rooftop mechanical and electrical equipment shall be screened from public view by building elements that are designed as an integral part of the building architecture, or by a parapet wall.

**[See Commentary]**  
**[See References]**